



**PERMIT APPLICATION REVIEW FORM FOR
CABLE, CONDUIT, AND POLE LINE ACTIVITY
IN FORT BEND COUNTY**

**Fort Bend County
Engineering Department**
301 Jackson Suite 401
Richmond, Texas 77469
281.633.7500
Permits@fortbendcountytx.gov

<input checked="checked" type="checkbox"/>	Right of Way Permit
<input type="checkbox"/>	Commercial Driveway Permit

Permit No: 2018-23063

The following "Notice of Proposed Cable, Conduit, and/or Pole Line activity in Fort Bend County" and accompanying attachments have been reviewed and the notice conforms to appropriate regulations set by Commissioner's Court of Fort Bend County, Texas.

(1) COMPLETE APPLICATION FORM:

- | | |
|--|--|
| <input checked="checked" type="checkbox"/> | a. Name of road, street, and/or drainage ditch affected. |
| <input checked="checked" type="checkbox"/> | b. Vicinity map showing course of directions |
| <input checked="checked" type="checkbox"/> | c. Plans and specifications |

(2) BOND:

<input type="checkbox"/>	County Attorney, approval when applicable.		
<input checked="checked" type="checkbox"/>	Perpetual bond currently posted.	Bond No: [REDACTED]	Amount: \$50,000.00
<input type="checkbox"/>	Performance bond submitted.	Bond No: _____	Amount: _____
<input type="checkbox"/>	Cashier's Check	Check No: _____	Amount: _____

(3) DRAINAGE DISTRICT APPROVAL (WHEN APPLICABLE):

Drainage District Approval

Date

We have reviewed this project and agree it meets minimum requirements.

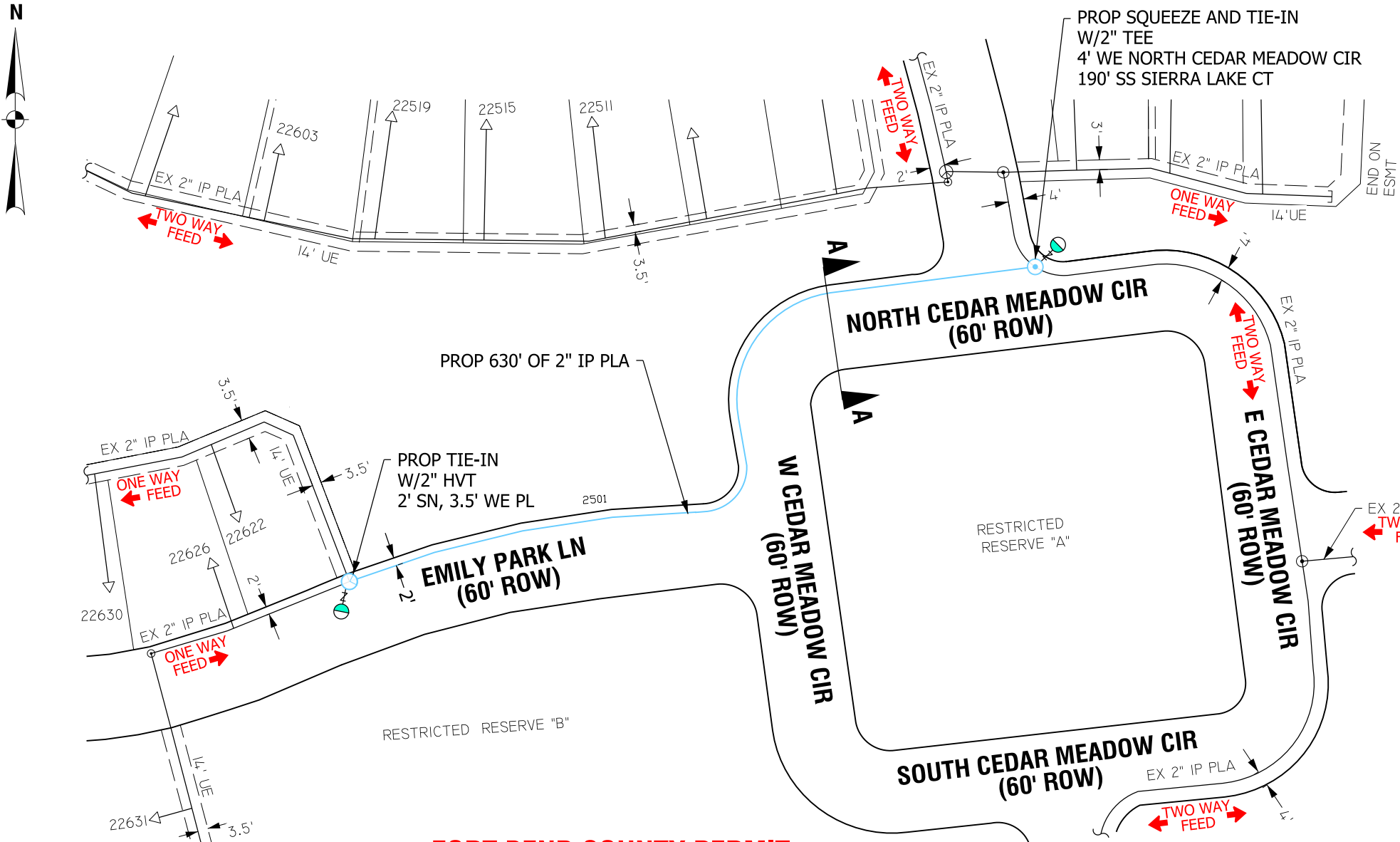


Permit Administrator

10/15/2018

Date

8/17/2018 2:16:44 PM M:\Josh Benitez\2018\2 - S\Hwy 99 at Westpark Hwy Tie-ins\North Cedar Meadow Cir - 2in pla Tie-in\EB5-181338.dgn



PERMITS REQUIRED FOR CONSTRUCTION

☐ NO PERMITS REQUIRED

☐ TXDOT

☐ CITY: _____

☒ COUNTY: FORT BEND

☐ FLOOD CONTROL/DRAINAGE: _____

☐ RAILROAD: _____

GAS STAKING

JOB NO: _____ BY: _____ DATE: _____

ESMTS. DED. BY: _____ ESMT. DOCUMENTS: _____

MONUMENTATION FND.: _____ PLAT DIST. CHK.=D: _____

ESMTS. NEEDED AT: _____

NON-STD. STAKING: _____ REASON: _____

LEGEND AND NOTES

EXISTING MAIN

PROP 2" IP PLA, SDR 11.0 630'

PE 2406/2708

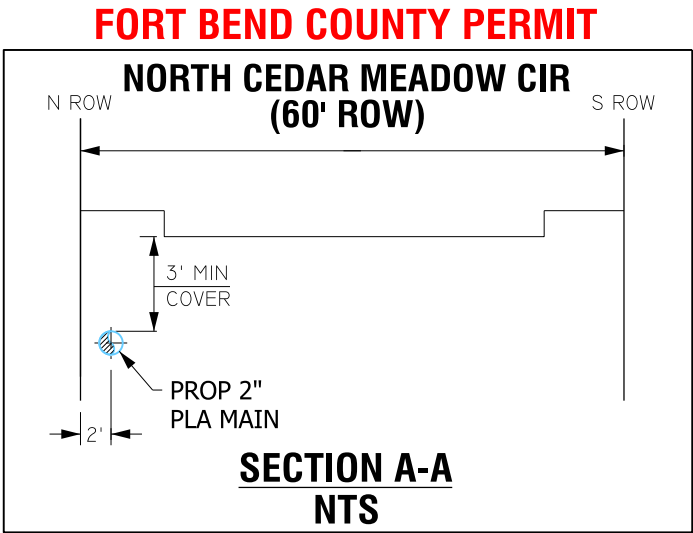
WO#: 87265707

PROP 2" TEE

PROP 5# ZINC ANODE

GCO#: 40269

STAKING REQUEST#: 87265707



GENERAL NOTES

1. FIELD VERIFY & LOCATE ALL EXISTING FEEDS, MAINS, AND SERVICES.
2. MAINTAIN MIN DEPTH OF 3' UNLESS OTHERWISE NOTED.
3. TEST PRESSURE @ 100 PSIG IN ACCORDANCE WITH SECTION CS-B-1.220 OF THE CONSTRUCTION & SERVICE MANUAL.
4. USE GAUGES TO MONITOR & MAINTAIN FEEDS, AND PRESSURE.
5. ALL SERVICE LINES MUST HAVE AN EFV OR CURB VALVE INSTALLED IN ACCORDANCE WITH SECTION CS-FORM 1.150 OF THE CONSTRUCTION AND SERVICE MANUAL AND EFV SIZING CHART. CONTACT ENGINEERING FOR ADDITIONAL SIZING RECOMMENDATIONS.
6. CONTACT ENGINEERING (JOSH BENITEZ 713-207-6072) WITH ANY NECESSARY FIELD CHANGES.
7. THIS PROJECT IS SUBJECT TO SEWER LATERAL INSPECTION TO VERIFY CLEARANCE, PER THE OPTIONS STATED IN THE CONSTRUCTION AND SERVICE MANUAL. ENGINEERING WAS UNABLE TO VERIFY THE CLEARANCE WITH MAPS AND RECORDS.

GENERAL NOTES:

CRITERIA TO BE USED FOR TRACER WIRE SELECTION WHEN INSTALLING PLASTIC GAS LINES

1- USE #14 TRACER WIRE FOR ALL RESIDENTIAL SERVICE LINES

2- USE #14 TRACER WIRE FOR SHORT BORES UP TO 300' AND ALL OTHER NON-BORE INSTALLATIONS

3- USE #10 TRACER WIRE WITH ALL BORES LONGER THAN 300'

4- USE #8 TRACER WIRE AS NEEDED FOR LARGE BAYOU CROSSINGS AND OTHER EXTRAORDINARY SITUATIONS AND COMPLEX BORES

INSPECTOR APPROVAL REQUIRED PRIOR TO WORK

DESIGNED BY: JOSH BENITEZ	713-207-6072
ESTIMATED COST: \$	CONTRIBUTION: \$
START DATE: 10/15/18	COMPLETION DATE: 10/19/18
PURPOSE AND NECESSITY: TO TIE-IN 2" IP PLA MAIN TO INCREASE PRESSURE DURING COLD WEATHER	

RECOMMENDED BY:	DATE:
SHAUNA WEINMANN	8/16/18
APPROVED BY:	DATE:

DRAWN BY: WJT	DATE: 8/16/18
SCALE: 1:100	SHEET: 1 OF 1

F:30240694O:30240688KM:525-G

LAMBERT:4455C4Z:248SZ:078TC:N/ASOG:08850

COLD WEATHER TIE-IN

AT NORTH CEDAR MEADOW CIR

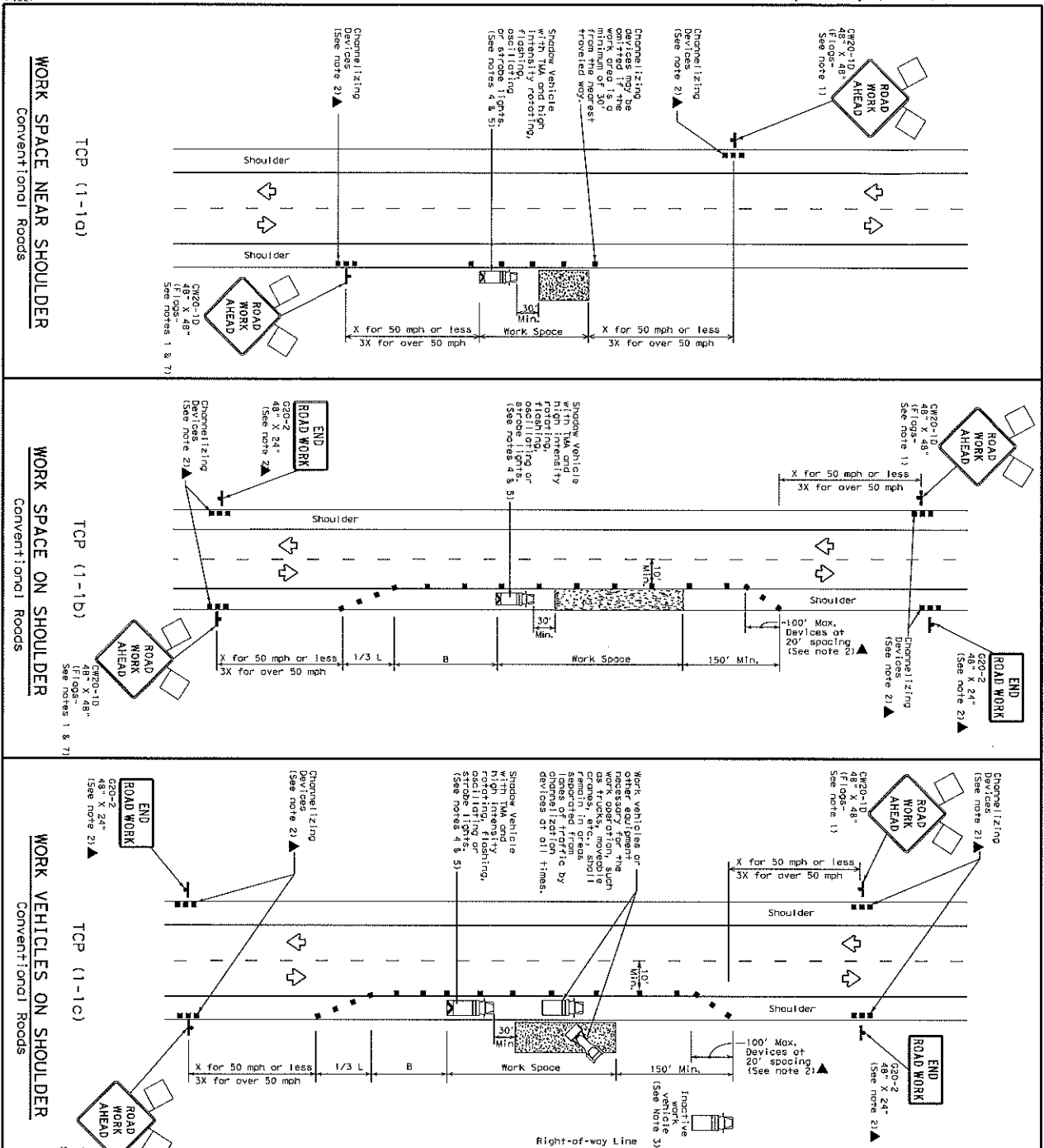
KATY, TEXAS

CenterPoint Energy

Texas Region - Houston Gas Engineering

DWG No: EB5-181338

DATE:
FILE:



LEGEND	
	Type 3 Barricade
	Heavy Work Vehicle
	Traffic Mounted Board
	Flashing Arrow Board
	Sign
	Flag
	Channelizing Device
	Traffic Mounted Arrowboard (TMA)
	Variable Message Sign (VMS)
	Traffic Flow
	Flagger

Posterior Formula No.	Minimum Number of Taper Lengths *	Suggested Length from Chopine Devices	Minimum Stitch Spacing Between "B" Sp-	Suggested Length from Chopine Devices
30	10 ¹ , 11 ¹ , 12 ¹	On a Taper	120	90"
35	13 ¹ , 15 ¹ , 18 ¹	30"	60	120
40	15 ¹ , 18 ¹ , 21 ¹	35"	70	150
45	20 ¹ , 25 ¹ , 32 ¹	40"	80	240
50	25 ¹ , 32 ¹ , 40 ¹	45	90	320
55	30 ¹ , 40 ¹ , 50 ¹	50"	100	400
60	40 ¹ , 50 ¹ , 60 ¹	60"	110	500
65	50 ¹ , 60 ¹ , 70 ¹	70"	120	600
70	60 ¹ , 70 ¹ , 80 ¹	80"	130	700
75	70 ¹ , 80 ¹ , 90 ¹	90"	140	800
80	80 ¹ , 90 ¹ , 100 ¹	100"	150	900
85	90 ¹ , 100 ¹ , 110 ¹	110"	160	1000
90	100 ¹ , 110 ¹ , 120 ¹	120"	170	1100
95	110 ¹ , 120 ¹ , 130 ¹	130"	180	1200
100	120 ¹ , 130 ¹ , 140 ¹	140"	190	1300
105	130 ¹ , 140 ¹ , 150 ¹	150"	200	1400
110	140 ¹ , 150 ¹ , 160 ¹	160"	210	1500
115	150 ¹ , 160 ¹ , 170 ¹	170"	220	1600
120	160 ¹ , 170 ¹ , 180 ¹	180"	230	1700
125	170 ¹ , 180 ¹ , 190 ¹	190"	240	1800
130	180 ¹ , 190 ¹ , 200 ¹	200"	250	1900
135	190 ¹ , 200 ¹ , 210 ¹	210"	260	2000
140	200 ¹ , 210 ¹ , 220 ¹	220"	270	2100
145	210 ¹ , 220 ¹ , 230 ¹	230"	280	2200
150	220 ¹ , 230 ¹ , 240 ¹	240"	290	2300
155	230 ¹ , 240 ¹ , 250 ¹	250"	300	2400
160	240 ¹ , 250 ¹ , 260 ¹	260"	310	2500
165	250 ¹ , 260 ¹ , 270 ¹	270"	320	2600
170	260 ¹ , 270 ¹ , 280 ¹	280"	330	2700
175	270 ¹ , 280 ¹ , 290 ¹	290"	340	2800
180	280 ¹ , 290 ¹ , 300 ¹	300"	350	2900
185	290 ¹ , 300 ¹ , 310 ¹	310"	360	3000
190	300 ¹ , 310 ¹ , 320 ¹	320"	370	3100
195	310 ¹ , 320 ¹ , 330 ¹	330"	380	3200
200	320 ¹ , 330 ¹ , 340 ¹	340"	390	3300
205	330 ¹ , 340 ¹ , 350 ¹	350"	400	3400
210	340 ¹ , 350 ¹ , 360 ¹	360"	410	3500
215	350 ¹ , 360 ¹ , 370 ¹	370"	420	3600
220	360 ¹ , 370 ¹ , 380 ¹	380"	430	3700
225	370 ¹ , 380 ¹ , 390 ¹	390"	440	3800
230	380 ¹ , 390 ¹ , 400 ¹	400"	450	3900
235	390 ¹ , 400 ¹ , 410 ¹	410"	460	4000
240	400 ¹ , 410 ¹ , 420 ¹	420"	470	4100
245	410 ¹ , 420 ¹ , 430 ¹	430"	480	4200
250	420 ¹ , 430 ¹ , 440 ¹	440"	490	4300
255	430 ¹ , 440 ¹ , 450 ¹	450"	500	4400
260	440 ¹ , 450 ¹ , 460 ¹	460"	510	4500
265	450 ¹ , 460 ¹ , 470 ¹	470"	520	4600
270	460 ¹ , 470 ¹ , 480 ¹	480"	530	4700
275	470 ¹ , 480 ¹ , 490 ¹	490"	540	4800
280	480 ¹ , 490 ¹ , 500 ¹	500"	550	4900
285	490 ¹ , 500 ¹ , 510 ¹	510"	560	5000
290	500 ¹ , 510 ¹ , 520 ¹	520"	570	5100
295	510 ¹ , 520 ¹ , 530 ¹	530"	580	5200
300	520 ¹ , 530 ¹ , 540 ¹	540"	590	5300
305	530 ¹ , 540 ¹ , 550 ¹	550"	600	5400
310	540 ¹ , 550 ¹ , 560 ¹	560"	610	5500
315	550 ¹ , 560 ¹ , 570 ¹	570"	620	5600
320	560 ¹ , 570 ¹ , 580 ¹	580"	630	5700
325	570 ¹ , 580 ¹ , 590 ¹	590"	640	5800
330	580 ¹ , 590 ¹ , 600 ¹	600"	650	5900
335	590 ¹ , 600 ¹ , 610 ¹	610"	660	6000
340	600 ¹ , 610 ¹ , 620 ¹	620"	670	6100
345	610 ¹ , 620 ¹ , 630 ¹	630"	680	6200
350	620 ¹ , 630 ¹ , 640 ¹	640"	690	6300
355	630 ¹ , 640 ¹ , 650 ¹	650"	700	6400
360	640 ¹ , 650 ¹ , 660 ¹	660"	710	6500
365	650 ¹ , 660 ¹ , 670 ¹	670"	720	6600
370	660 ¹ , 670 ¹ , 680 ¹	680"	730	6700
375	670 ¹ , 680 ¹ , 690 ¹	690"	740	6800
380	680 ¹ , 690 ¹ , 700 ¹	700"	750	6900
385	690 ¹ , 700 ¹ , 710 ¹	710"	760	7000
390	700 ¹ , 710 ¹ , 720 ¹	720"	770	7100
395	710 ¹ , 720 ¹ , 730 ¹	730"	780	7200
400	720 ¹ , 730 ¹ , 740 ¹	740"	790	7300
405	730 ¹ , 740 ¹ , 750 ¹	750"	800	7400
410	740 ¹ , 750 ¹ , 760 ¹	760"	810	7500
415	750 ¹ , 760 ¹ , 770 ¹	770"	820	7600
420	760 ¹ , 770 ¹ , 780 ¹	780"	830	7700
425	770 ¹ , 780 ¹ , 790 ¹	790"	840	7800
430	780 ¹ , 790 ¹ , 800 ¹	800"	850	7900
435	790 ¹ , 800 ¹ , 810 ¹	810"	860	8000
440	800 ¹ , 810 ¹ , 820 ¹	820"	870	8100
445	810 ¹ , 820 ¹ , 830 ¹	830"	880	8200
450	820 ¹ , 830 ¹ , 840 ¹	840"	890	8300
455	830 ¹ , 840 ¹ , 850 ¹	850"	900	8400
460	840 ¹ , 850 ¹ , 860 ¹	860"	910	8500
465	850 ¹ , 860 ¹ , 870 ¹	870"	920	8600
470	860 ¹ , 870 ¹ , 880 ¹	880"	930	8700
475	870 ¹ , 880 ¹ , 890 ¹	890"	940	8800
480	880 ¹ , 890 ¹ , 900 ¹	900"	950	8900
485	890 ¹ , 900 ¹ , 910 ¹	910"	960	9000
490	900 ¹ , 910 ¹ , 920 ¹	920"	970	9100
495	910 ¹ , 920 ¹ , 930 ¹	930"	980	9200
500	920 ¹ , 930 ¹ , 940 ¹	940"	990	9300
505	930 ¹ , 940 ¹ , 950 ¹	950"	1000	9400
510	940 ¹ , 950 ¹ , 960 ¹	960"	1010	9500
515	950 ¹ , 960 ¹ , 970 ¹	970"	1020	9600
520	960 ¹ , 970 ¹ , 980 ¹	980"	1030	9700
525	970 ¹ , 980 ¹ , 990 ¹	990"	1040	9800
530	980 ¹ , 990 ¹ , 1000 ¹	1000"	1050	9900
535	990 ¹ , 1000 ¹ , 1010 ¹	1010"	1060	10000
540	1000 ¹ , 1010 ¹ , 1020 ¹	1020"	1070	10100
545	1010 ¹ , 1020 ¹ , 1030 ¹	1030"	1080	10200
550	1020 ¹ , 1030 ¹ , 1040 ¹	1040"	1090	10300
555	1030 ¹ , 1040 ¹ , 1050 ¹	1050"	1100	10400
560	1040 ¹ , 1050 ¹ , 1060 ¹	1060"	1110	10500
565	1050 ¹ , 1060 ¹ , 1070 ¹	1070"	1120	10600
570	1060 ¹ , 1070 ¹ , 1080 ¹	1080"	1130	10700
575	1070 ¹ , 1080 ¹ , 1090 ¹	1090"	1140	10800
580	1080 ¹ , 1090 ¹ , 1100 ¹	1100"	1150	10900
585	1090 ¹ , 1100 ¹ , 1110 ¹	1110"	1160	11000
590	1100 ¹ , 1110 ¹ , 1120 ¹	1120"	1170	11100
595	1110 ¹ , 1120 ¹ , 1130 ¹	1130"	1180	11200
600	1120 ¹ , 1130 ¹ , 1140 ¹	1140"	1190	11300
605	1130 ¹ , 1140 ¹ , 1150 ¹	1150"	1200	11400
610	1140 ¹ , 1150 ¹ , 1160 ¹	1160"	1210	11500
615	1150 ¹ , 1160 ¹ , 1170 ¹	1170"	1220	11600
620	1160 ¹ , 1170 ¹ , 1180 ¹	1180"	1230	11700
625	1170 ¹ , 1180 ¹ , 1190 ¹	1190"	1240	11800
630	1180 ¹ , 1190 ¹ , 1200 ¹	1200"	1250	11900
635	1190 ¹ , 1200 ¹ , 1210 ¹	1210"	1260	12000
640	1200 ¹ , 1210 ¹ , 1220 ¹	1220"	1270	12100
645	1210 ¹ , 1220 ¹ , 1230 ¹	1230"	1280	12200
650	1220 ¹ , 1230 ¹ , 1240 ¹	1240"	1290	12300
655	1230 ¹ , 1240 ¹ , 1250 ¹	1250"	1300	12400
660	1240 ¹ , 1250 ¹ , 1260 ¹	1260"	1310	12500
665	1250 ¹ , 1260 ¹ , 1270 ¹	1270"	1320	12600
670	1260 ¹ , 1270 ¹ , 1280 ¹	1280"	1330	12700
675	1270 ¹ , 1280 ¹ , 1290 ¹	1290"	1340	12800
680	1280 ¹ , 1290 ¹ , 1300 ¹	1300"	1350	12900
685	1290 ¹ , 1300 ¹ , 1310 ¹	1310"	1360	13000
690	1300 ¹ , 1310 ¹ , 1320 ¹	1320"	1370	13100
695	1310 ¹ , 1320 ¹ , 1330 ¹	1330"	1380	13200
700	1320 ¹ , 1330 ¹ , 1340 ¹	1340"	1390	13300
705	1330 ¹ , 1340 ¹ , 1350 ¹	1350"	1400	13400
710	1340 ¹ , 1350 ¹ , 1360 ¹	1360"	1410	13500
715	1350 ¹ , 1360 ¹ , 1370 ¹	1370"	1420	13600
720	1360 ¹ , 1370 ¹ , 1380 ¹	1380"	1430	13700
725	1370 ¹ , 1380 ¹ , 1390 ¹	1390"	1440	13800
730	1380 ¹ , 1390 ¹ , 1400 ¹	1400"	1450	13900
735	1390 ¹ , 1400 ¹ , 1410 ¹	1410"	1460	14000
740	1400 ¹ , 1410 ¹ , 1420 ¹	1420"	1470	14100
745	1410 ¹ , 1420 ¹ , 1430 ¹	1430"	1480	14200
750	1420 ¹ , 1430 ¹ , 1440 ¹	1440"	1490	14300
755	1430 ¹ , 1440 ¹ , 1450 ¹	1450"	1500	14400
760	1440 ¹ , 1450 ¹ , 1460 ¹	1460"	1510	14500
765	1450 ¹ , 1460 ¹ , 1470 ¹	1470"	1520	14600
770	1460 ¹ , 1470 ¹ , 1480 ¹	1480"	1530	14700
775	1470 ¹ , 1480 ¹ , 1490 ¹	1490"	1540	14800
780	1480 ¹ , 1490 ¹ , 1500 ¹	1500"	1550	14900
785	1490 ¹ , 1500 ¹ , 1510 ¹	1510"	1560	15000
790	1500 ¹ , 1510 ¹ , 1520 ¹	1520"	1570	15100
795	1510 ¹ , 1520 ¹ , 1530 ¹	1530"	1580	15200
800	1520 ¹ , 1530 ¹ , 1540 ¹	1540"	1590	15300
805	1530 ¹ , 1540 ¹ , 1550 ¹	1550"	1600	15400
810	1540 ¹ , 1550 ¹ , 1560 ¹	1560"	1610	15500
815	1550 ¹ , 1560 ¹ , 1570 ¹	1570"	1620	15600
820	1560 ¹ , 1570 ¹ , 1580 ¹	1580"	1630	15700
825	1570 ¹ , 1580 ¹ , 1590 ¹	1590"	1640	15800
830	1580 ¹ , 1590 ¹ , 1600 ¹	1600"	1650	15900
835	1590 ¹ , 1600 ¹ , 1610 ¹	1610"	1660	16000
840	1600 ¹ , 1610 ¹ , 1620 ¹	1620"	1670	16100
845	1610 ¹ , 1620 ¹ , 1630 ¹	1630"	1680	16200

*** Taper lengths have been rounded off.
L=length of Taper (FT) W=Width of Offset (FT) S=Posted Speed(MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

GENERAL NOTES

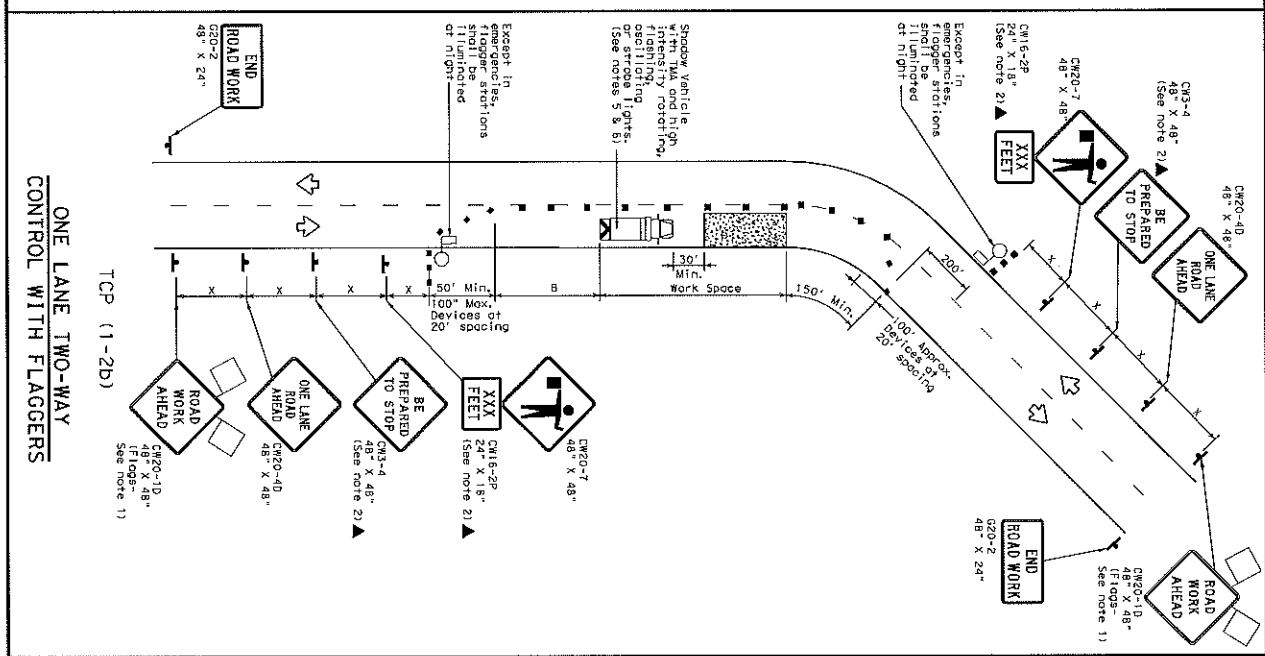
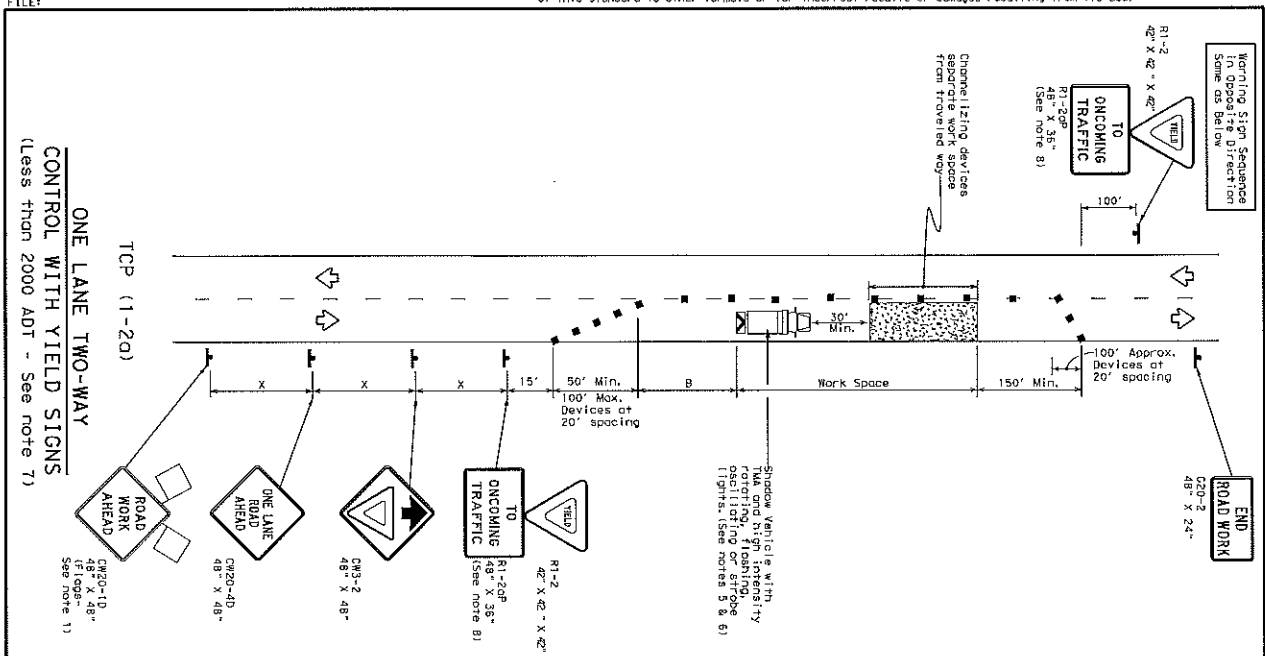
2. All traffic control devices illustrated on the RECORD, except those denoted with the following symbol may be omitted when stored in a standard position on the plans, or for routine maintenance work, when approved by the Engineer.
3. Traffic work vehicles or other equipment should be parked near the right-of-way line and not parked on the paved shoulder.
4. A shadow vehicle with a TM should be used every time it can be positioned 10 to 100 feet in advance of the area of work exposure without unduly interfering with traffic.
5. When a shadow vehicle is used, the following conditions must be met:
 - a. The vehicle must be clearly visible to the traffic control devices to remain in place.
 - b. Type 3 Barricades or other channelizing devices may be substituted for the Shadow Vehicle and TM.
 - c. Additional traffic vehicles with TM's may be positioned off the pavement to protect those vehicles in place.
 - d. Surface text to those vehicles in TM's may protect work spaces.
6. Shoulder work on divided highways, interchanges and roundabouts.
7. CH2-15 "SHOULDER WORK" signs may be used in place of CH20-1D "ROAD WORK AHEAD" signs for shoulder work on conventional roadways.

TRAFFIC CONTROL PLAN
CONVENTIONAL ROAD
SHOULDER WORK

FILE#		4021-1-B, DON		SN#		CR#		INV#		EST#
PROJECT		DECEMBER 1985		CN#	5472	JOB		SHEET#		
REV#S/DKS										
2-94		1-98								
6-95		2-12								
1-97		2-18								
				B137		CHART		SHEET NO.		
151										

DISCLAIMER: The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE:
FILE:



GENERAL NOTES








1. Flares attached to signs where shown are REQUIRED.
2. All traffic to control device illustrated or REQUIRED, except those otherwise with the sign, shall be controlled by the sign.
3. The CH-3 "BE PREPARED TO STOP" sign may be installed after the CH20-40 "ONE LANE ROAD AHEAD" sign, but proper sign spacing shall be maintained.
4. Signs indicating any kind of stop sign should be maintained at least 1500 feet apart. Signs indicating a stop sign should be maintained at least 1500 feet apart. Signs indicating a stop sign should be maintained at least 1500 feet apart.
5. A Stop sign with a TMA should be used only if it can be positioned 50 to 100 feet in advance of the area of an exposure without adversely affecting the performance or quality of the work. If workers are not proper, present but road or work conditions require a stop sign, a stop sign should be used.
6. Additional Stop sign vehicles with TMA may be positioned on the paved surface, next to those shown in order to protect work area spaces.

TCP (1-20)

1. R-12 YIELD traffic control may be used on projects with approaches that have adequate sight distance, for projects in urban areas, wet seasons should not longer than 100 feet, and for conditions with less than 350 ft/s work.
2. R-12 YIELD sign with R-12-20 "DO NOT ENTER" plaque shall be placed on a support or a 7 foot minimum mounting height.
3. ICP 11-201
4. Flagger should use two-way radios or other methods of communication to control traffic.
5. Length of work zones should be based on the ability of flaggers to communicate.
6. If the work spaces is located near a non-control or vertical curve, the buffer distances should be increased in order to maintain adequate stopping sight distance to the flagger and a clear view of the work zone.
7. Flagger should be positioned in the centerline of the road when a pilot car is leading traffic and approved by the Engineer.
8. Flagger should use 24" STOP/SLOW paddles to control traffic. Flags should be limited to emergency situations.

TCP (1-2b)

- [illegible]


LEGEND	
	Type 3 Barrier
	Heavy Work Vehicle
	Truck Mounted Attenuator (TMA)
	Portable Changeable Message Sign (PCMS)
	Traffic Flow
	Flagger
	Flag

Posed Spaced	Formula	Adm. man Tolerance **	Suggested Spacing Device	Max. man Tolerance Device	Primum Spacing Device	Suggested Spacing Device			
*		10"	11"	12"	13"	14"			
30		150"	165"	180"	30"	60"	120"	90"	200"
35	$\frac{W}{6}$	205"	225"	245"	35"	70"	140"	120"	250"
40	$\frac{W}{8}$	265"	295"	320"	40"	80"	160"	155"	305"
45		450"	495"	540"	45"	90"	320"	195"	350"
50		500"	550"	600"	50"	100"	400"	240"	425"
55	$\frac{L}{4} = W/5$	550"	605"	660"	55"	110"	500"	295"	495"
60		600"	660"	720"	60"	120"	600"	350"	570"
65		650"	715"	780"	65"	130"	700"	410"	645"
70		700"	770"	840"	70"	140"	800"	475"	730"
75		750"	825"	900"	75"	150"	900"	540"	820"

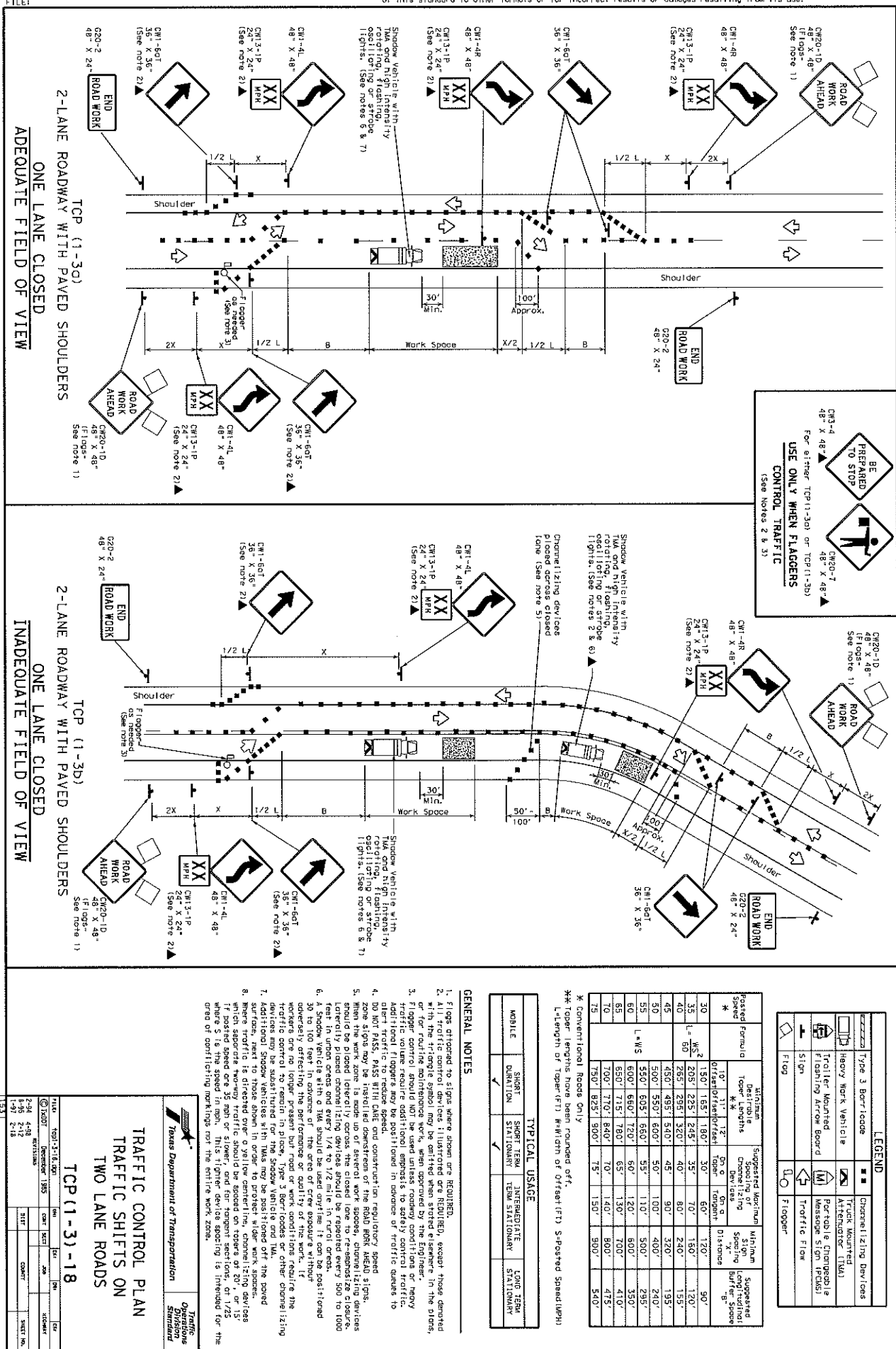
```
* Conventional Roads Only
** Taper lengths have been rounded off.
l=length of Taper(FT) w=width of Offset(FT) S=Posted Speed(MPH)
```


















TYPICAL IMAGE

FISCAL CODE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

 Texas Department of Transportation		Operations Division Scheduling	
<h1>TRAFFIC CONTROL PLAN</h1> <h2>ONE-LANE TWO-WAY</h2> <h2>TRAFFIC CONTROL</h2>			
<h3>TCP (1-2) - 18</h3>			
FILE# 1-27-2-18, 180	DATE 12/27/2018	TIME 1:37	PAGE 2-18
PROJECT 4-30 4-18 2-96 2-12 1-37 2-15	DISTRICT 01ST COUNTY SHELBY NO.	STATE 01ST COUNTY SHELBY NO.	SHEET NO. 18

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LEGEND	
	Type 3 Bortlecode
	Heavy Mark Vehicle
	Trailer Mounted Board
	Fishing Net
	Sign
	Floor
	Flower
	Flower
	Flower
	Flower
	Flower
	Flower
	Flower
	Flower
	Flower
	Flower
	Flower

Posed Formula Space	Minimum Distance to Top Layer Lengths	Suggested Maximum Distance to Top Layer Lengths	Minimum Significance	Suggested Maximum Significance
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**Taper lengths have been rounded off.
L=length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

TYPICAL USAGE				
MOBILE	SHORT DURATION	SHORT TERM STATIONARY	INTERMEDIATE TERM STATIONARY	LONG TERM STATIONARY
	✓	✓		

GENERAL NOTES

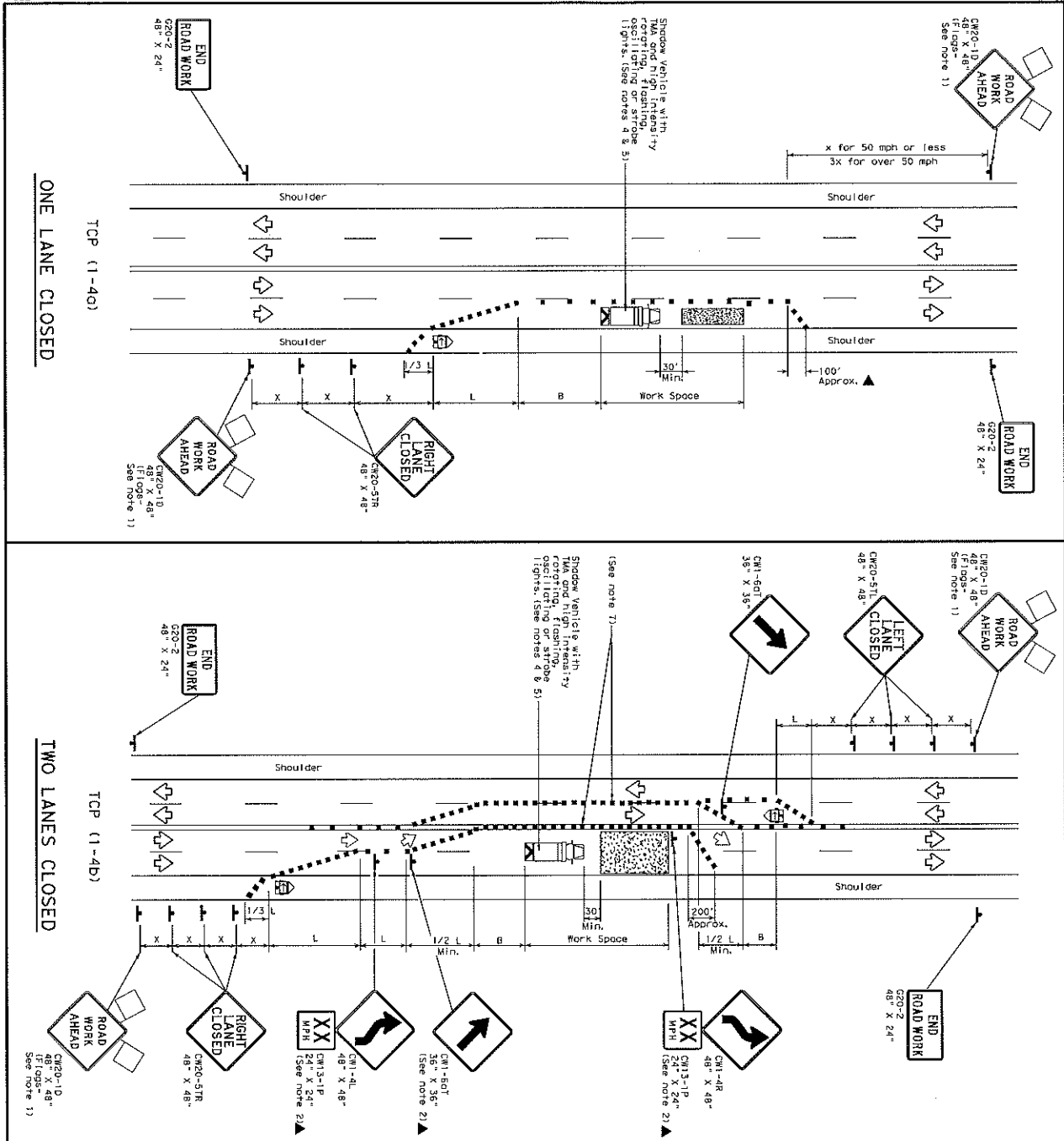
1. Flaps traffic to signals where shown are REQUIRED, except those denoted by a "P" (priority) signal.
2. Flaps to the right of the signal may be omitted when the signal is in the "B" (Bypass) position.
3. Flap or control should not be used unless roadway conditions or heavy traffic volume require an additional emphasis to safely control traffic.
4. Additional flaps may be positioned in advance of traffic signals to:
 - a. Warn of a change in traffic control.
 - b. Warn of a change in construction or regulatory speed zone signs.
 - c. Warn of a change in the designated destination of the ROAD WORK AHEAD signs.
5. When the work zone is made up of several work spaces, changing devices should be placed liberally across the closed lane to re-emphasize closure.
6. Flaps should be placed in advance of the closed lane at intervals of 500 to 1000 feet in urban areas and every 1/4 to 1/2 mile in rural areas.
7. A Shadow Vehicle with a TMA should be used only if it can be positioned 30 to 100 feet in advance of the area of work space without adversely affecting the performance or quality of the work. If the traffic is stopped, the Shadow Vehicle should be positioned in the work zone.
8. Flaps may be substituted for the Shadow Vehicle and TMA.
9. Additional Shadow Vehicles with TMAs may be positioned at the posted advance, next to those shown in order to protect older work spaces.
10. When a vehicle is stopped traffic should be stopped on topers of 20' or 1/5 thereof if the speed is 10 mph, 30' or 1/3 thereof if the speed is 20 mph, or 45' or 1/3 thereof if the speed is 30 mph. This 1/3 rule also applies to the area of conflicting movements for the traffic work zone.

TRAFFIC CONTROL PLAN
TRAFFIC SHIFTS ON
TWO LANE ROADS

 Texas Department of Transportation		Traffic Division Standard	
<h2 style="text-align: center;">TRAFFIC CONTROL PLAN</h2> <h3 style="text-align: center;">TRAFFIC SHIFTS ON TWO LANE ROADS</h3>			
<h1 style="font-size: 2em;">TCP (1-3) - 18</h1>			
DATE 12/01/2011 1:30 1:45 1:57	TIME 12:01-1:00 PM December 1, 2011 12:01 PM 12:15 PM 12:30 PM	DIST. 1.00 1.00 1.00	SHEET NO. 1 2 3
COUNTY TARRANT		SHEET NO. 1 2 3	

DISCLAIMER:
The use of this standard is governed by the "Texas Engineering Practice Act". No warranty of any kind is made by TxDOT for any purpose whatsoever. TxDOT assumes no responsibility for the conversion of this standard to other formats or for incorrect results or damages resulting from its use.

DATE:
FILE:



LEGEND		Channelizing Devices	
	Type 3 Barricade		Truck Mounted Attenuator (TMA)
	Trailer Mounted Flashing Arrow Board		Portable Channelized Mass Sign (PMS)
	Sign		Traffic Flow
	Floodgate		

Posted Speed	Minimum Topper Lengths	Suggested Maximum Spacing of Topper Devices	Minimum Spacing of Topper Devices	Suggested Maximum Spacing of Topper Devices
30	150'	180'	30'	60'
35	205'	245'	35'	70'
40	265'	295'	40'	80'
45	325'	355'	45'	90'
50	385'	415'	50'	100'
55	445'	475'	55'	110'
60	505'	535'	60'	120'
65	565'	595'	65'	130'
70	625'	655'	70'	140'
75	685'	715'	75'	150'

* Conventional Roads Only
* Topper lengths have been rounded off.
* Length of Topper (FT) Width of Offset (FT) S-Posted Speed (MPH)

TYPICAL USAGE			
MOBILE	SHORT TERM STATIONARY	INTERMEDIATE	LONG TERM STATIONARY
SMART	SMART	SMART	SMART
BARRELS	BARRELS	BARRELS	BARRELS
CONES	CONES	CONES	CONES
WARRIORS	WARRIORS	WARRIORS	WARRIORS
SAFETY	SAFETY	SAFETY	SAFETY

GENERAL NOTES

- Flare attached to signs where shown are REQUIRED.
- All traffic control devices illustrated are REQUIRED, except those denoted with the triangle symbol may be omitted when stored elsewhere in the plans, or for routine maintenance work, when approved by the Engineer.
- The C20-10 Road Work Ahead sign may be replaced by the C20-11 Road Work Ahead sign.
- A Shoulder Vehicle with a TMA should be used only if it can be positioned 30 to 100 feet in advance of the area of crew exposure without adversely affecting the performance or quality of the work. If workers are no longer in place, the Shoulder Vehicle with a TMA should be removed.
- Additional Shoulder Vehicles with TMAs may be positioned off the paved surface, next to those shown in order to protect wider work spaces.

TCP 1-40

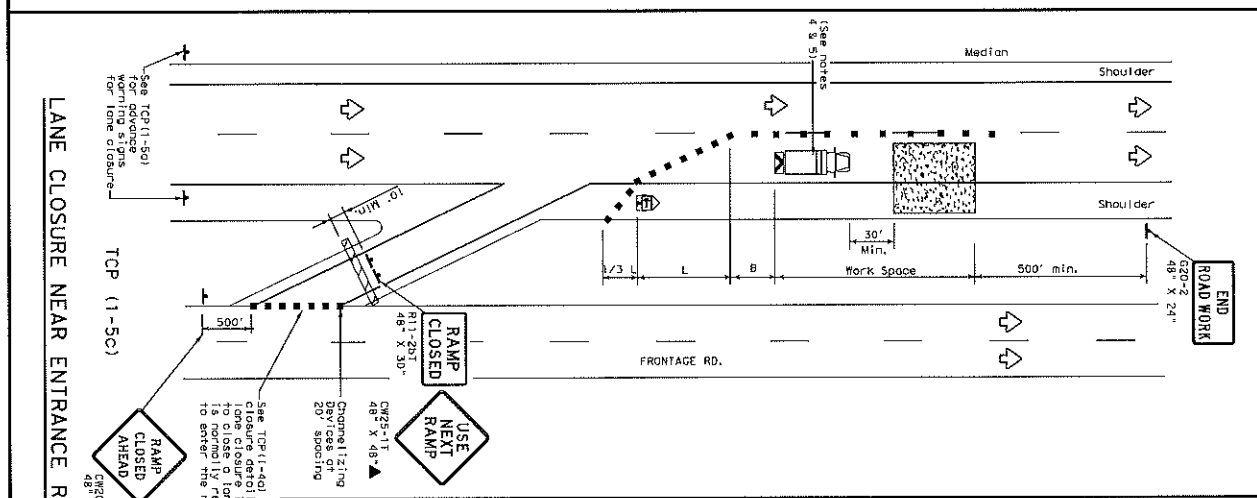
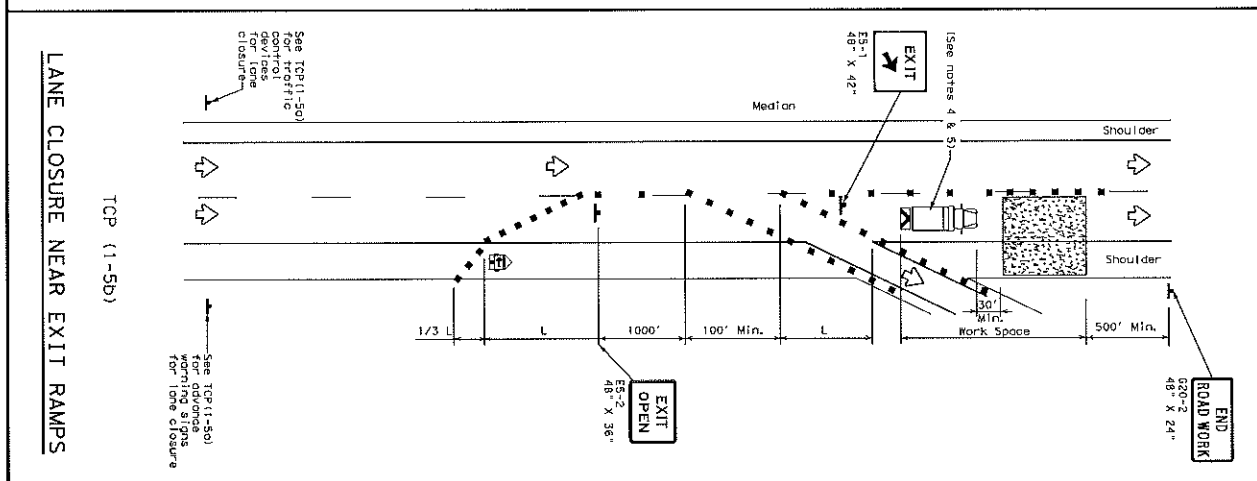
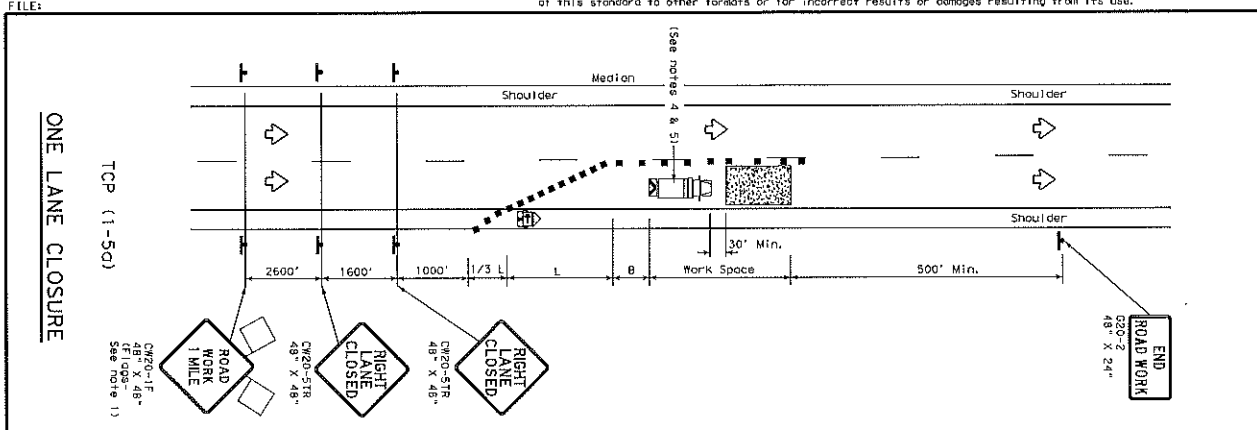
5. If this TCP is used for a left lane closure, C20-51, "LEFT LANE CLOSED" signs shall be used and channelizing devices shall be placed on the centerline where needed to protect the work space from opposing traffic with the arrow panel placed in the closed lane near the end of the merging taper.










TCP 1-4b

7. Where traffic is directed over a yellow centerline, channelizing devices which separate two-way traffic should be spaced on uppers of 20' or 15' where S is the speed in mph. This taper device spacing is intended for the grade of conflicting markings, not the entire work zone.

TRAFFIC CONTROL PLAN		Operations Division Signature	
LANE CLOSURES ON MULTILANE CONVENTIONAL ROADS			
TCP 1-4-18			
DATE	TIME	DATE	TIME
2-24-18	4:58	DATE	TIME
1-21-18	2:18	DATE	TIME

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LEWEND			
	Type 3 Bertronde		Channelizing Devices
	Heavy Work Vehicle		Truck Mounted Attenuator (TMA)
	Trailer Mounted Floating Arrow Board		Portable Changeable Message Sign (PCMS)
	Sign		Traffic Flow
	Flag		Flagger

Posed Formula	Minimum Top Layer Lentils	Suggested Locking Device	Minimum Sign Location	Suggested Buffer Length
#	10" 11" 12"	On a Tablet	Distance	"
30	1350 165 160	30'	60'	120'
30	1350 165 160	30'	60'	120'
35	205 225 245	35'	70'	140'
40	285 295 320	40'	80'	240'
45	450 495 540	45'	90'	320'
50	500 540 600	50'	100'	420'
55	550 605 660	55'	110'	500'
60	600 660 720	60'	120'	600'
65	715 780	65'	130'	700'
70	700 770 840	70'	140'	800'
75	825 825 900	75'	150'	900'

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** Taper lengths have been rounded off.
L=length of Taper (FT)  W=Width of Offset (FT)  S=Posted Speed (MPH)


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L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

L=Length of Taper (FT) W=Width of Offset (FT) S=Posted Speed (MPH)

GENERAL NOTES

1. Lamps attached to signs where shown, are REQUIRED, except those attached to signs that are illuminated by the sign itself.
2. All traffic control devices illustrated are REQUIRED, except those devoted with the following: a) signs that are illuminated by the sign itself; b) signs that are illuminated by the sign itself; c) signs that are illuminated by the sign itself; d) signs that are illuminated by the sign itself; e) signs that are illuminated by the sign itself; f) signs that are illuminated by the sign itself; g) signs that are illuminated by the sign itself; h) signs that are illuminated by the sign itself; i) signs that are illuminated by the sign itself; j) signs that are illuminated by the sign itself; k) signs that are illuminated by the sign itself; l) signs that are illuminated by the sign itself; m) signs that are illuminated by the sign itself; n) signs that are illuminated by the sign itself; o) signs that are illuminated by the sign itself; p) signs that are illuminated by the sign itself; q) signs that are illuminated by the sign itself; r) signs that are illuminated by the sign itself; s) signs that are illuminated by the sign itself; t) signs that are illuminated by the sign itself; u) signs that are illuminated by the sign itself; v) signs that are illuminated by the sign itself; w) signs that are illuminated by the sign itself; x) signs that are illuminated by the sign itself; y) signs that are illuminated by the sign itself; z) signs that are illuminated by the sign itself.
3. Channelizing devices used to close lanes may be supplemented with the Diamond Alignment Sign placed on every other channelizing device. Channeling may be extended to plastic drums, or per the standards.
4. Use of flashing lights is required on all vehicles used for traffic control, including but not limited to: a) vehicles used for traffic control; b) vehicles used for traffic control; c) vehicles used for traffic control; d) vehicles used for traffic control; e) vehicles used for traffic control; f) vehicles used for traffic control; g) vehicles used for traffic control; h) vehicles used for traffic control; i) vehicles used for traffic control; j) vehicles used for traffic control; k) vehicles used for traffic control; l) vehicles used for traffic control; m) vehicles used for traffic control; n) vehicles used for traffic control; o) vehicles used for traffic control; p) vehicles used for traffic control; q) vehicles used for traffic control; r) vehicles used for traffic control; s) vehicles used for traffic control; t) vehicles used for traffic control; u) vehicles used for traffic control; v) vehicles used for traffic control; w) vehicles used for traffic control; x) vehicles used for traffic control; y) vehicles used for traffic control; z) vehicles used for traffic control.
5. Additional Shadow Vehicle with TMA may be positioned in each closed lane, on the shoulder or off the paved surface, next to those shown in order to protect a wider work space.

 <p>Texas Department of Transportation</p>	<p>Traffic Operations Division Standard</p>
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TRAFFIC CONTROL PLAN LANE CLOSURES FOR DIVIDED HIGHWAYS

TCP (1-5) -18

PLATA	1601-B-18.dgn	3011	EX	0011	5011
© 19001	Feb 04 2012	0001	SECT	JOB	1400000001
2-13	AKACTIONS	0111		0200017	00017 0001